

| REVISIONS |      |         |    |
|-----------|------|---------|----|
| No.       | DATE | COMMENT | BY |
|           |      |         |    |
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|           |      |         |    |

**PROTECT YOURSELF**

PROJECT No.: AJ17240  
 DRAWN BY: JZ  
 CHECKED BY: JH  
 DATE: 4/10/2020  
 SCALE: 1" = 30'  
 CAD I.D.: ANJ17240\_SSP

PROJECT:  
 HIGHWAY IMPROVEMENT PLANS  
 FOR  
 McDonald's USA, LLC



4295 & 4299 US ROUTE 130  
 EDGEWATER PARK TOWNSHIP  
 BURLINGTON COUNTY, NEW JERSEY

NOT APPROVED FOR CONSTRUCTION

**ATLANTIC**  
TRAFFIC+DESIGN

35 Technology Drive  
 Warren, New Jersey 07059  
 908-769-5588  
 NJ Certificate of Authorization No. 24GAZ7957900

**JOHN R. HARTER**

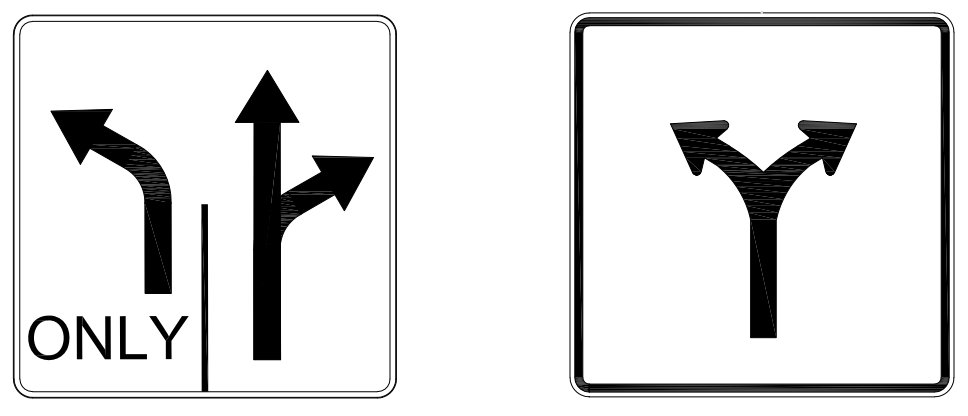
PROFESSIONAL ENGINEER  
 NEW JERSEY LICENSE NO. 41033

SHEET TITLE:  
**SIGNING & STRIPING PLAN**

SHEET NUMBER:  
 1  
 OF 1

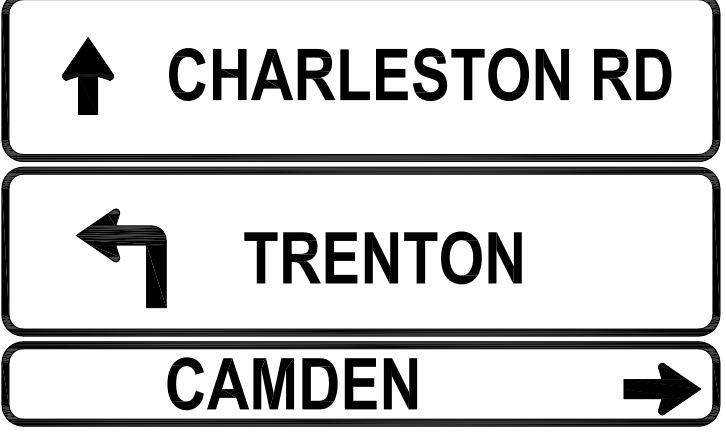
REVISION 0 4/10/2020

**SIGN LEGEND:**  
N.T.S.



R(NJ)3-8F  
30" x 30"

R(NJ)3-8EE  
30" x 30"



D3-2 (MOD A)  
NOT TO SCALE

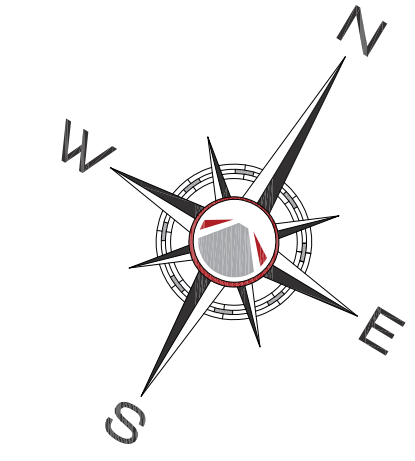
| MUTCD DESIGNATION | SIGN LEGEND                | SIZE            |
|-------------------|----------------------------|-----------------|
| R1-1              | STOP                       | 36" x 36"       |
| M1-4              | US ROUTE SIGN (130)        | 24" x 24"       |
| M3-3              | CARDINAL DIRECTION (SOUTH) | 24" x 12"       |
| R1-2              | YIELD                      | 36" x 36" x 36" |
| R2-1              | SPEED LIMIT SIGN           | 30" x 36"       |
| R3-2              | NO LEFT TURN               | 24" x 24"       |
| R3-5              | RIGHT TURN ONLY            | 30" x 36"       |
| R5-1              | DO NOT ENTER               | 36" x 36"       |
| R6-1L             | ONE WAY (LEFT)             | 36" x 12"       |
| R6-1R             | ONE WAY (RIGHT)            | 36" x 12"       |

**PROPOSED STRIPING LEGEND:**

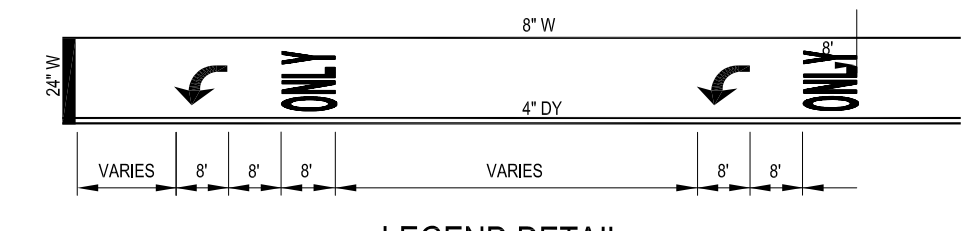
- 4" W = 4" WHITE LINE
- 4" W BROKEN = 4" WHITE BROKEN LINE, 10' LINE/30' GAP
- 4" W DOTTED = 4" WHITE DOTTED LINE
- 8" W = 8" WHITE LINE
- 24" W = 24" WHITE LINE
- 4" W = 4" YELLOW LINE
- 4" DY = 4" DOUBLE YELLOW LINE
- 4" DY DOTTED = 4" DOUBLE YELLOW DOTTED LINE
- 24" W GORE = 24" WHITE LINE, 12" O-C @ 45°

**GENERAL NOTES:**

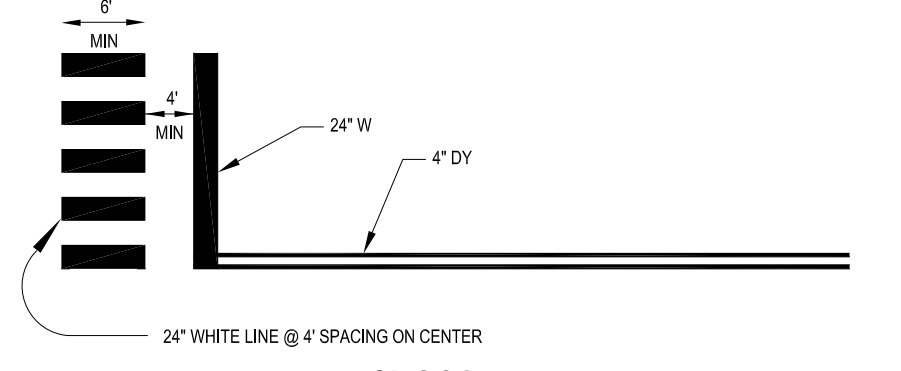
- ALL EXISTING SIGNING AND STRIPING IN CONFLICT WITH THE PROPOSED DESIGN SHALL BE REMOVED.
- ALL PROPOSED SIGNING AND STRIPING SHALL CONFORM TO THE CURRENT MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES BY THE FEDERAL HIGHWAY ADMINISTRATION.
- PAVEMENT STRIPES AND MARKINGS SHALL BE INSTALLED IN ACCORDANCE WITH NJDOT REQUIREMENTS.
- PAVEMENT STRIPES AND MARKINGS SHALL BE LONG LIFE PAVEMENT MARKINGS.
- REMOVAL OF EXISTING PAVEMENT MARKINGS WILL BE REMOVED BY HYDRO BLASTING METHOD OR MILL/PAVE.



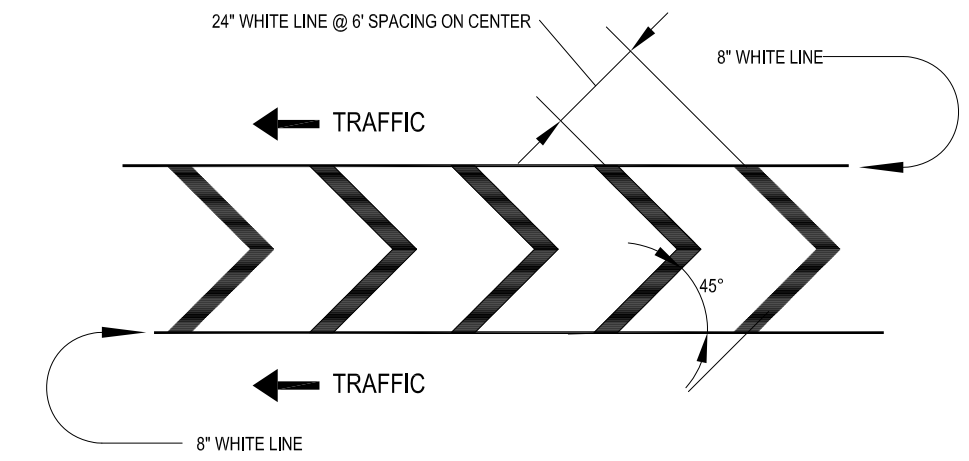
**STRIPING DETAILS**  
N.T.S.



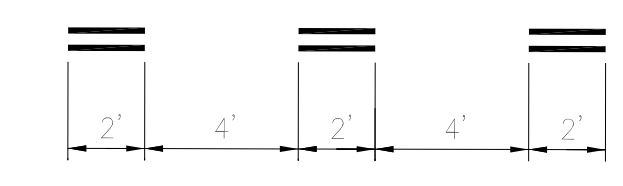
LEGEND DETAIL



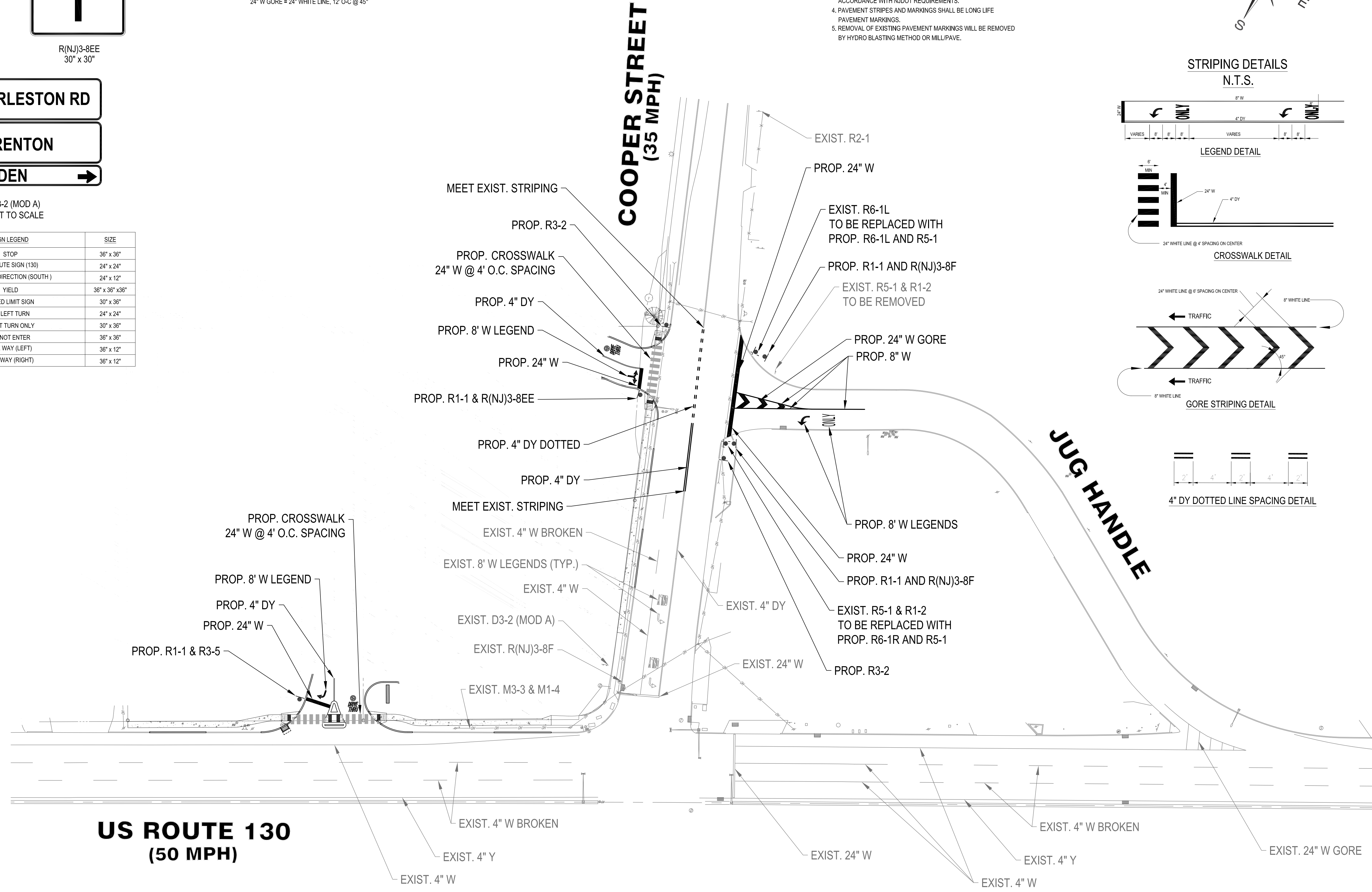
CROSSWALK DETAIL



GORE STRIPING DETAIL



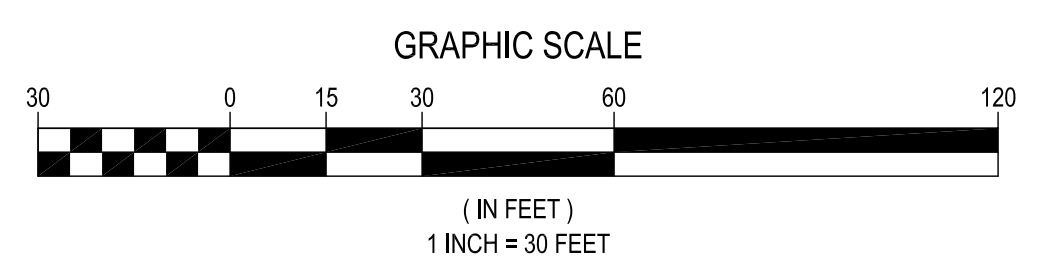
4" DY DOTTED LINE SPACING DETAIL



**US ROUTE 130**  
**(50 MPH)**

**COOPER STREET**  
**(35 MPH)**

**JUG HANDLE**

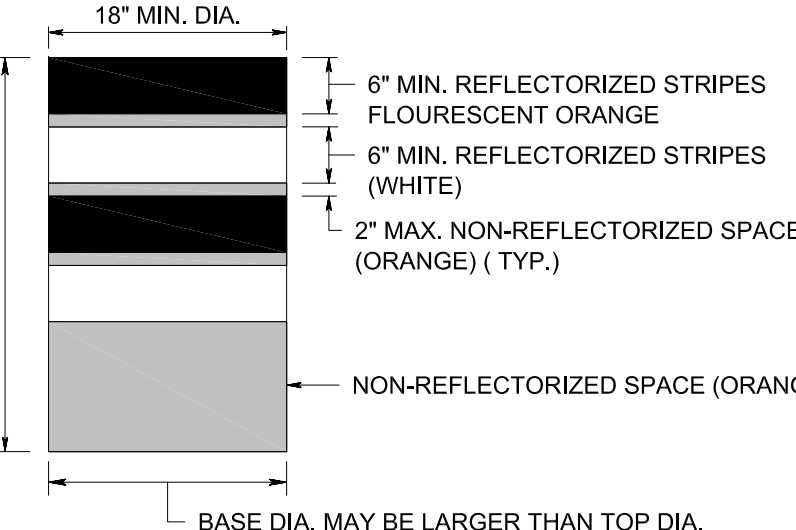


THE CONTRACTOR IS RESPONSIBLE FOR VERIFYING THE EXACT LOCATION AND DEPTH OF UTILITIES, PIPING, DRYWELLS, ETC. PRIOR TO THE START OF WORK. THE CONTRACTOR MUST CONTACT THE APPROPRIATE LOCAL ONE CALL SYSTEM TO ENSURE THAT ALL UTILITIES ARE PROPERLY AND COMPLETELY MARKED OUT IN THE FIELD PRIOR TO ANY WORK AT THE SITE. ANY DISCREPANCIES BETWEEN THE FIELD LOCATIONS AND THE PLANS SHALL BE REPORTED TO THE ENGINEER IMMEDIATELY. THE CONTRACTOR IS ADVISED THAT UTILITY INFORMATION SHOWN ON THE PLAN IS A COMPILATION OF FIELD LOCATIONS BY VARIOUS UTILITY AGENCIES AND RECORD DRAWINGS AND MUST BE CONFIRMED ACCORDINGLY. THE CONTRACTOR IS ALSO ADVISED THAT ALL SUCH FACILITIES DISTURBED DURING CONSTRUCTION MUST BE REPAIRED OR REPLACED AT THE CONTRACTOR'S EXPENSE.

GENERAL NOTES:

- 1. ADVANCE WARNING SIGNS DISTANCES, AND TAPER LENGTHS MAY BE EXTENDED, AT DIRECTION OF THE DEPARTMENT, TO ADJUST FOR REDUCED VISIBILITY DUE TO HORIZONTAL AND VERTICAL CURVATURE OF THE ROADWAY.
2. THE APPROXIMATE LOCATIONS OF THE ILLUMINATED FLASHING ARROW BOARDS ARE SHOWN ON THE TRAFFIC CONTROL PLANS. THESE LOCATIONS MAY BE MODIFIED AS APPROVED BY RE. TO ADJUST FOR VISIBILITY DUE TO HORIZONTAL OR VERTICAL CURVATURE OF THE ROADWAY OR TO POSITION AT A SAFER LOCATION. ILLUMINATED FLASHING ARROW BOARDS ARE TO BE USED FOR TEMPORARY LANE CLOSINGS AND AT LOCATIONS SHOWN ON THE TRAFFIC CONTROL PLANS.
3. PRIOR TO ANY ROAD CONSTRUCTION, TRAFFIC CONTROL SIGNS AND DEVICES SHALL BE IN PLACE.
4. RAMP AND/OR SIDE STREETS ENTERING THE ROADWAY AFTER THE FIRST ADVANCE WARNING SIGN SHALL BE PROVIDED WITH AT LEAST ONE W20-IF SIGN (ROAD WORK AHEAD) AS A MINIMUM.
5. ALL EXISTING ROAD SIGNS, PAVEMENT MARKINGS AND/OR FLOWABLE PAVEMENT REFLECTORS WHICH CONFLICT WITH THE PROPOSED TRAFFIC CONTROL PLAN SHALL BE COVERED, REMOVED OR RELOCATED AS DIRECTED BY THE RE.
6. CONFLICTING OR NON-OPERATING SIGNAL INDICATIONS ON EITHER THE EXISTING, TEMPORARY, OR PROPOSED TRAFFIC SIGNAL SYSTEMS SHALL BE BAGGED OR COVERED.
7. MAINTENANCE AND PROTECTION OF TRAFFIC SHALL BE IN ACCORDANCE WITH THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES - PART VI "STANDARDS AND GUIDES FOR TRAFFIC CONTROL FOR STREET AND HIGHWAY CONSTRUCTION, MAINTENANCE, UTILITY, AND INCIDENT MANAGEMENT OPERATIONS", UNLESS OTHERWISE NOTED IN THE PLANS AND SPECIFICATIONS.
8. CONSTRUCTION SIGN W99-2 (GIVE US A BRAKE) SHALL BE LOCATED 200 FEET IN ADVANCE OF PROJECT LIMITS.
9. A W1-6 (ARROW) SIGN MOUNTED ON A BREAKAWAY BARRICADE AND CENTERED ON THE CLOSED WIDTH SHALL BE LOCATED 100 FEET BEYOND EACH INTERSECTION OR MAIN ACCESS POINT WITHIN THE AREA OF A LANE OR SHOULDER CLOSURE.
10. CONSTRUCTION SIGNS R11-4 (ROAD CLOSED TO THRU TRAFFIC) SHALL BE PLACED AT THE INTERSECTING STREETS WHICH ARE CLOSED TO TRAFFIC BECAUSE OF CONSTRUCTION.
11. CONSTRUCTION SIGNS W8-9A (SYMBOL FOR UNEVEN PAVEMENT) AND W8-14A (GROOVED PAVEMENT) SHALL BE USED WHEN SUCH PAVEMENT CONDITIONS EXIST. THE PLACEMENT OF THESE SIGNS SHALL BE AS DIRECTED BY THE RE.
12. MOVING WORK AREAS IN A LANE CLOSURE REQUIRE A TRAILER MOUNTED ILLUMINATED FLASHING ARROW TO REMAIN AT THE END OF THE TAPER, THE TRAFFIC CONTROL TRUCK WITH MOUNTED CRASH CUSHION THAT SHALL MOVE WITH THE WORK AREAS TO KEEP A 70 FEET MIN. AND 150 FEET MAX. BUFFER IN ADVANCE OF EACH WORK AREA.
13. THE CONTRACTOR SHALL SUBMIT A PLAN FOR THE SAFE ACCESS OF CONSTRUCTION VEHICLES THROUGHOUT THE WORK SITE WHERE SPACE CONSTRAINTS PREVENT THE USE OF LANE CLOSURES. THE PLAN SHALL BE SUBMITTED TO THE RE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS.
14. ALL EXCAVATED AREAS WITHIN OR ADJACENT TO THE ROADWAY SHALL BE BACKFILLED AND PLACED ON AT LEAST 6H : 1V SLOPE BEFORE THE END OF EACH WORK DAY. OTHER EXCAVATED AREA WITHIN THE CLEAR ZONE SHALL BE BACKFILLED.
15. WHERE REQUIRED, THE CONTRACTOR SHALL MAKE PROVISIONS FOR MAINTAINING PEDESTRIAN CROSSING LOCATIONS AND TYPE AS DIRECTED BY THE RE.
16. BITUMINOUS CONCRETE PLACED DURING THE VARIOUS CONSTRUCTION STAGES SHALL BE TRANSITIONED ON A MINIMUM 20H : 1V SLOPE TO MEET THE ADJACENT EXISTING GRADE AT THE LONGITUDINAL AND TRANSVERSE LIMITS OF THE STAGE CONSTRUCTION AREAS UNLESS OTHERWISE NOTED ON THE STAGE CONSTRUCTION PLANS.
17. THE PLACEMENT AND OR RELOCATION OF PRECAST CONCRETE CURB, CONSTRUCTION BARRIER SHALL BE DONE DURING APPROVED OFF-PEAK HOURS WHEN TRAFFIC MAY BE REDUCED TO ONE LANE IN EACH DIRECTION.
18. CONSTRUCTION ZONE SPEED LIMIT WILL BE DETERMINED BY THE TRAFFIC SIGNAL & SAFETY ENGINEERING, REGIONAL TRAFFIC ENGINEER - WORK ZONE, AT THE TIME OF OR DURING CONSTRUCTION, AS REQUESTED BY THE R.E.
19. THE SPEED LIMIT, R2-1 (BLACK ON WHITE) WITH ADDED WORK ZONE PLATE (BLACK ON ORANGE) SIGNS SHALL BE LOCATED THROUGH WORK AREAS AS DIRECTED BY THE TRAFFIC SIGNAL & SAFETY ENGINEERING REGIONAL TRAFFIC ENGINEER - WORK ZONE.
20. THE REDUCED SPEED AHEAD SIGN, W3-5(S) (BLACK ON ORANGE) SHALL BE LOCATED IN ADVANCE OF SPEED LIMIT R2-1 SIGNS WHICH REDUCE THE NORMAL POSTED SPEED LIMIT THROUGH THE CONSTRUCTION ZONE.
21. TRAFFIC FINES DOUBLED IN WORK AREA R(N)5-17(S) , 4 FEET BY 2.5 FEET SIGN SHALL BE LOCATED 500 FEET AFTER THE FIRST ADVANCE WARNING SIGN, (W20 SERIES) AT EACH WORK AREA LOCATED WITHIN URBAN AREAS. THIS SIGN SHALL ALSO BE USED ON PROJECTS REQUIRING MOVING OPERATIONS IN WHICH CASE THE SIGN SHALL BE MOUNTED ON A SLOW MOVING CONSTRUCTION VEHICLE.
22. THE FINAL HMA SURFACE PAVEMENT SHALL NOT BE CONSTRUCTED UNTIL THE FINAL STAGE OF THE PROJECT UNLESS OTHERWISE DIRECTED BY THE RE OR INDICATED ON THE PLANS. MANHOLES AND INLETS SHALL BE SET TO FINISHED GRADE AND TEMPORARY PAVEMENT RAMP ARE TO BE CONSTRUCTED AROUND THEM WITH A MINIMUM 20H : 1V SLOPE IN ALL DIRECTIONS USING HOT MIX ASPHALT PAVEMENT. THIS TEMPORARY MATERIAL WILL BE REMOVED IMMEDIATELY PRIOR TO PLACING THE SURFACE COURSE.
23. TRAFFIC CONTROL DEVICES FOR LANE CLOSURES INCLUDING SIGNS, CONES, BARRICADES, ETC. SHALL BE PLACED AS SHOWN ON PLANS. SIGNS SHALL NOT BE PLACED WITHOUT ACTUAL LANE CLOSURES AND SHALL BE IMMEDIATELY REMOVED UPON REMOVAL OF THE CLOSURES.
24. CONES MAY BE SUBSTITUTED FOR DRUMS AND INSTALLED UPON THE APPROVAL OF THE RE.

- 25. TRAFFIC IMPACT NOTICES AND CHANGES
A. TERMS:
WHEN THE FOLLOWING TERMS ARE USED, THE INTENT AND MEANING SHALL BE AS FOLLOWS:
i. IMPACTS TO NORMAL TRAFFIC FLOW - WORK THAT REQUIRES A PORTION OF THE PAVED ROADWAY BEING BLOCKED OR CLOSED WITH SAFETY DEVICES OR VEHICLES, INCLUDING, BUT NOT LIMITED TO, FULL OR PARTIAL LANE CLOSURES, FULL OR PARTIAL RAMP CLOSURES, SHOULDER CLOSURES, MOVING OPERATIONS SUCH AS TRAFFIC STRIPING OR SWEEPING, LANE SHIFTS, OR ALTERNATING TRAFFIC. THIS APPLIES EVEN WHEN DETOURS ARE PROVIDED.
ii. TEMPORARY LANE CLOSURES - WORK DESCRIBED UNDER "IMPACTS TO NORMAL TRAFFIC FLOW" WHICH IS ROUTINELY SET UP AND REMOVED ON A DAILY BASIS.
iii. PERMANENT LANE CLOSURES - WORK DESCRIBED UNDER "IMPACTS TO NORMAL TRAFFIC FLOW" WHICH REMAINS IN PLACE CONTINUOUSLY FOR 24 HOURS OR MORE.
B. ADVANCE NOTICES
FOR THE INITIAL START OF WORK THAT REQUIRES "IMPACTS TO NORMAL TRAFFIC FLOW", THE CONTRACTOR SHALL NOTIFY THE RE IN WRITING, ON THE ADVANCE FORM TO-103 PROVIDED BY THE DEPARTMENT, OF THE PROPOSED DATE. THE NOTICE SHALL BE SUBMITTED AT LEAST TWENTY-EIGHT CALENDAR DAYS, BUT NOT MORE THAN SIXTY CALENDAR DAYS, BEFORE THE PROPOSED DATE. START OF WORK THAT IMPACTS NORMAL TRAFFIC FLOW WILL NOT BE PERMITTED PRIOR TO THE DATE STATED IN THE NOTICE. THE CONTRACTOR SHALL CONFIRM, IN WRITING TO THE RE, THE PROPOSED DATE SEVEN (AND/OR FOURTEEN) CALENDAR DAYS BEFORE STARTING THE ESTABLISHMENT OF THE TRAFFIC CONTROL MEASURES FOR THE TRAFFIC IMPACT. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE RE IF THE PROPOSED ESTABLISHMENT CAN NOT BE COMPLETED ON THE PROPOSED DATE.
FOR A "PERMANENT LANE CLOSURE", THE CONTRACTOR SHALL NOTIFY THE RE IN WRITING, ON ADVANCE FORM TO-103 OF THE PROPOSED DATE A NEW TRAFFIC PATTERN WILL BE ESTABLISHED. THE NOTICE SHALL BE SUBMITTED AT LEAST TWENTY-EIGHT CALENDAR DAYS, BUT NOT MORE THAN SIXTY CALENDAR DAYS, IN ADVANCE OF THE PROPOSED DATE. START OF A NEW TRAFFIC PATTERN WILL NOT BE PERMITTED PRIOR TO THE DATE STATED IN THE NOTICE. THE CONTRACTOR SHALL CONFIRM, IN WRITING TO THE RE, THE PROPOSED DATE OF THE NEW TRAFFIC PATTERN SEVEN (AND/OR FOURTEEN) DAYS BEFORE STARTING TRAFFIC CONTROL MEASURES FOR THE ESTABLISHMENT OF THE NEW PATTERN. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE RE IF THE PROPOSED ESTABLISHMENT CAN NOT BE COMPLETED ON THE PROPOSED DATE.
THE HOURS FOR THE ESTABLISHMENT OF A NEW TRAFFIC PATTERN ARE THE SAME AS FOR SINGLE LANE TRAFFIC CLOSURE HOURS.
ADVANCE NOTICES SENT PRIOR TO THE PRE-CONSTRUCTION MEETING SHALL BE ADDRESSED TO THE CONTACT PERSON AS SPECIFIED IN SUBSECTION 101.04 OF THE SPECIAL PROVISIONS.
C. PROGRESS NOTICES
ALL "IMPACTS TO NORMAL TRAFFIC FLOW" SCHEDULED FOR THE SEVEN DAY PERIOD STARTING ON THE FOLLOWING MONDAY SHALL BE SUBMITTED TO THE RE BY 9:00 AM OF EACH FRIDAY ON WEEKLY FORM TO-100 PROVIDED BY THE DEPARTMENT.
EACH DAY OF "TEMPORARY LANE CLOSURES" SHALL BE SUBMITTED TO THE RE BY 9:00 AM ON THE DAY IN ADVANCE OF THE START OF THOSE OPERATIONS ON DAILY FORM TO-101 PROVIDED BY THE DEPARTMENT.
"TEMPORARY LANE CLOSURES" FOR WEEKENDS SHALL BE SUBMITTED TO THE RE BY 9:00 AM ON THE IMMEDIATELY PRECEDING FRIDAY ON THE DAILY FORM TO-101 PROVIDED BY THE DEPARTMENT.
D. CHANGES TO THE SCHEDULED CLOSURES
REQUEST FOR A CHANGE TO THE TRAFFIC CONTROL REQUIREMENTS IN THE CONTRACT DOCUMENTS SHALL BE SUBMITTED IN WRITING TO THE RE AS FOLLOWS:
CHANGES TO THE SCHEDULED HOURS FOR "TEMPORARY LANE CLOSURES" SHALL BE SUBMITTED TO THE R.E. AT LEAST EIGHT CALENDAR DAYS IN ADVANCE OF WHEN THE CHANGE IS PROPOSED TO START.
OTHER PROPOSED CHANGES TO "TEMPORARY LANE CLOSURES" AND ALL CHANGES TO "PERMANENT LANE CLOSURES" SHALL BE SUBMITTED TO THE RE AS SPECIFIED IN THE SPECIAL PROVISIONS.
DRUMS SHALL BE MADE OF ORANGE PLASTIC WITH A MINIMUM OF FOUR ALTERNATE FLUORESCENT ORANGE AND SILVER (WHITE) RETROREFLECTIVE STRIPES. IF THERE ARE NON-REFLECTORIZED SPACES BETWEEN THE STRIPES, THEY SHALL BE NO MORE THAN 2" WIDE. RETROREFLECTIVE SHEETING FOR STRIPES SHALL CONFORM WITH ASTM D 4956 S2 REQUIREMENTS AND SHALL BE FROM THE APPROVED PRODUCTS LIST MAINTAINED BY THE BUREAU OF QUALITY MANAGEMENT SERVICES, NEW TECHNOLOGIES AND PRODUCTS SECTION.
THE TOP OF THE DRUM SHALL NOT BE OPEN. DRUMS SHALL BE CONSTRUCTED TO INHIBIT ROLLING IF KNOCKED OVER.
THE REFLECTORIZED AREA OF DRUMS SHALL BE ROUND EXCEPT THAT OTHER SHAPES, WHICH PROVIDE THE SAME VISIBILITY AS AN 18 INCH DIAMETER ROUND DRUM REGARDLESS OF ORIENTATION, MAY BE USED IF APPROVED BY THE BUREAU OF MATERIALS.
WHEN BALLAST IS REQUIRED BY THE ENGINEER, SAND SHALL BE USED. THE MAXIMUM WEIGHT OF THE BALLAST SHALL BE 50 LBS. AND BE LOCATED APPROXIMATELY AT GROUND LEVEL. ALTERNATE TYPES OF BALLAST SHALL BE APPROVED BY THE ENGINEER.
E. CONES
CONES SHALL BE MADE OF ORANGE PLASTIC WITH A MINIMUM OF FOUR ALTERNATE FLUORESCENT ORANGE AND SILVER (WHITE) RETROREFLECTIVE STRIPES. IF THERE ARE NON-REFLECTORIZED SPACES BETWEEN THE STRIPES, THEY SHALL BE NO MORE THAN 2" WIDE. RETROREFLECTIVE SHEETING FOR STRIPES SHALL CONFORM WITH ASTM D 4956 S2 REQUIREMENTS AND SHALL BE FROM THE APPROVED PRODUCTS LIST MAINTAINED BY THE BUREAU OF QUALITY MANAGEMENT SERVICES, NEW TECHNOLOGIES AND PRODUCTS SECTION.
CONES SHALL BE CONSTRUCTED TO INHIBIT ROLLING IF KNOCKED OVER.
THE REFLECTORIZED AREA OF CONES SHALL BE ROUND EXCEPT THAT OTHER SHAPES, WHICH PROVIDE THE SAME VISIBILITY AS AN 18 INCH DIAMETER ROUND CONE REGARDLESS OF ORIENTATION, MAY BE USED IF APPROVED BY THE BUREAU OF MATERIALS.
WHEN BALLAST IS REQUIRED BY THE ENGINEER, SAND SHALL BE USED. THE MAXIMUM WEIGHT OF THE BALLAST SHALL BE 50 LBS. AND BE LOCATED APPROXIMATELY AT GROUND LEVEL. ALTERNATE TYPES OF BALLAST SHALL BE APPROVED BY THE ENGINEER.



DRUMS

TRAFFIC CONES

LEGEND

- BREAKAWAY BARRICADES
BREAKAWAY BARRICADES WITH LIGHT STOCK SIGN (0.024")
CONSTRUCTION SIGNS
DRUMS
CONE
DIRECTION OF TRAFFIC FLOW
FLAGGER
ILLUMINATED FLASHING ARROW MOUNTED ON TOWING VEHICLE SHOWING CAUTION MODE
ILLUMINATED FLASHING ARROW MOUNTED ON TOWING VEHICLE SHOWING ARROW PATTERN (Left, Right, Both)
TRAFFIC CONTROL TRUCK WITH MOUNTED CRASH CUSHION AND ARROW BOARD SHOWING CAUTION MODE
TRAFFIC CONTROL TRUCK WITH MOUNTED CRASH CUSHION AND ARROW BOARD SHOWING ARROW PATTERN (Left, Right, Both)
TEMPORARY CRASH CUSHION, INERTIAL BARRIER SYSTEM
TEMPORARY CRASH CUSHION, (all other approved)
BUFFER ZONE
WORK AREA
PAINT STRIPING TRUCK OR OTHER OPERATING VEHICLE

1.ALLOWABLE LANE CLOSURE HOURS

ROUTE 130 SB, MP: 42.98 - (THREE TRAVEL LANES - SECTION)
NOVEMBER 15 TO JANUARY 05

Table with columns for lane closure status (All Lanes Maintained, Two Lanes Maintained - One Lane Closed, One Lane Maintained - Two Lanes Closed) and days/hours.

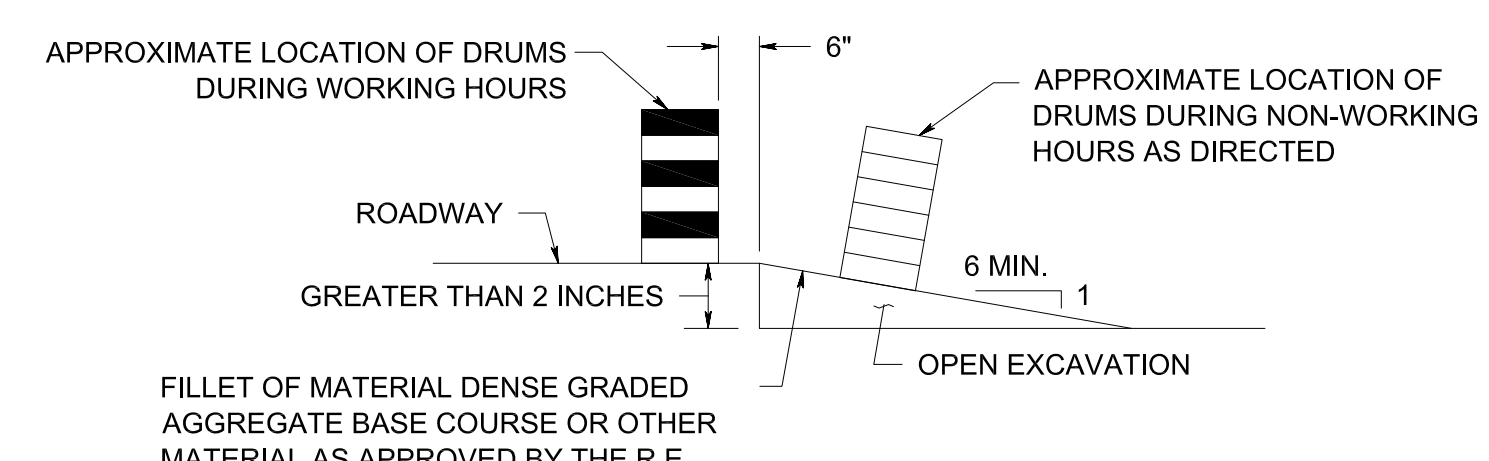
2. NO TEMPORARY LANE CLOSURES, RAMP CLOSURES, SHOULDER CLOSURES, OR TRAFFIC SHIFT WILL BE PERMITTED ON THE FOLLOWING HOLIDAYS:

Table listing holidays and their corresponding lane closure restrictions (e.g., Easter Sunday, Memorial Day, etc.).

NOTES:

- TRAFFIC CONES SHALL BE PREDOMINATELY ORANGE IN COLOR.
BASES MAY BE OF BREAKAWAY BALLASTED TYPE.
MINOR MANUFACTURER'S VARIATIONS MAY BE ACCEPTABLE UPON APPROVAL OF THE ENGINEER.
LANE CLOSURES MUST BE COORDINATED WITH OTHER PROJECTS THAT MAY BE UNDERWAY AT THE SAME TIME NEAR THE PROJECT AREA.
SHOULDER CLOSURE WILL BE PERMITTED ANYTIME AS LONG AS CLOSURE DOES NOT IMPACT TRAVEL LANES. HOLIDAY RESTRICTIONS WILL APPLY.
ACCESS TO ALL DRIVEWAYS AND FIRE LANES MUST BE MAINTAINED AT ALL TIMES DURING BUSINESS HOURS AND ONE DRIVEWAY AND ALL FIRE LANES MUST BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION.
FOR TRAFFIC SHIFTS DURING CONSTRUCTION, SIGNS WILL BE NEEDED TO DESIGNATE BUSINESS DRIVEWAYS TO MINIMIZE TRAFFIC CONFUSION.
RAMP / AUXILIARY LANES / SIDE STREETS MAY BE CLOSED BETWEEN THE HOURS OF 10:00 PM AND 5:00 AM THE NEXT DAY UNLESS OTHERWISE SPECIFIED. AN APPROVED SIGNED DETOUR IS REQUIRED FOR ALL RAMP / AUXILIARY LANES / SIDE STREETS CLOSURES. TWO CONSECUTIVE RAMP / AUXILIARY LANES / SIDE STREETS MAY NOT BE CLOSED AT THE SAME TIME.
A TRAFFIC MANAGEMENT PLAN WILL BE REQUIRED FOR THE PROJECT IN ACCORDANCE WITH THE NJDOT TRAFFIC MITIGATION GUIDELINES, BDC14T-01.
PLEASE PROVIDE PORTABLE VARIABLE MESSAGE SIGNS OF WHOSE LOCATIONS AND MESSAGES WILL BE INCLUDED AS PART OF THE TRAFFIC MITIGATION PLAN.
SHOULDERS SHALL BE OPEN FROM 7:00 AM TO 9:00 AM AND 3:00 PM TO 7:00 PM FOR MORNING AND EVENING RUSH HOURS.
CONTRACTOR TO REQUEST FOR MARK-OUT BY TRAFFIC OPERATIONS CENTER SOUTH PRIOR TO TEN (10) STATE BUSINESS DAYS OF COMMENCEMENT OF PROPOSED WORK.
VARIABLE MESSAGE SIGNS SHALL BE INSTALLED TWO (2) WEEKS PRIOR TO THE START OF CONSTRUCTION AT TWO (2) LOCATIONS: ALONG WESTBOUND NJSH ROUTE 130, 1500 FEET IN ADVANCE OF THE NJSH ROUTE 130 BUFFER AREA AND ALONG SOUTHBOUND HIGHLAND AVENUE, 1500 FEET IN ADVANCE OF ITS INTERSECTION WITH NJSH ROUTE 130. THE SIGNS SHALL DISPLAY THE FOLLOWING MESSAGES:

TRAFFIC CONES



NOTE:

- ESCAPE RAMPS MUST BE CONSTRUCTED AND MAINTAINED DURING NON-WORKING HOURS WHERE A VERTICAL DROP GREATER THAN 2 INCHES EXISTS ADJACENT TO TRAVELED LANE.

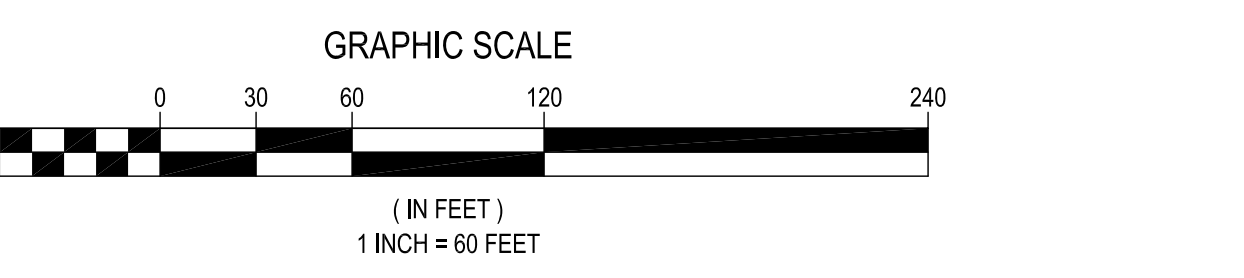
ESCAPE RAMP DETAIL

Table: RECOMMENDED TAPER LENGTH AND SPACING FOR CHANNELIZING TAPERS. Columns include Regulatory Approach Speed of Traffic, Minimum Taper Ratio, Minimum Taper Length, Maximum Device Spacing, and Maximum Device Spacing Along Tangents.

Table: RECOMMENDED SIGHT DISTANCE TO BEGINNING OF CHANNELIZING TAPERS. Columns include Regulatory Approach Speed of Traffic, Desirable Rural, Urban, and Minimum Rural and Urban Sight Distances.

NOTE:
THE MAXIMUM DEVICE SPACING ALONG CURVES SHALL BE AS DEFINED FOR TAPERS (B) IN THE ABOVE TABLE.

- NOTES:
1. AVOIDANCE MANEUVER IS FOR A SPEED, PATH, AND/OR DIRECTION CHANGE PRIOR TO THE BEGINNING OF CHANNELIZING TAPERS.
2. RECOMMENDED DISTANCES BETWEEN TWO SEPARATE LANE CLOSURES SHALL BE DOUBLE THE VALUES SHOWN ABOVE.
3. RURAL AND URBAN ROAD DESIGNATIONS SHALL BE AS DEFINED IN THE NJDOT STATE HIGHWAY STRAIGHT LINE DIAGRAMS.
4. DESIRABLE VALUES SHALL BE PROVIDED WHEREVER POSSIBLE. IF IT IS NOT FEASIBLE OR PRACTICAL TO PROVIDE DESIRABLE VALUES BECAUSE OF HORIZONTAL OR VERTICAL CURVATURE OR IF RELOCATION OF THE TAPER IS NOT POSSIBLE, THEN MINIMUM VALUES CAN BE APPLIED. WHEN MINIMUM VALUES ARE USED, SPECIAL ATTENTION SHOULD BE GIVEN TO THE USE OF SUITABLE TRAFFIC CONTROL DEVICES FOR PROVIDING ADVANCED WARNING OF THE CONDITIONS THAT ARE LIKELY TO BE ENCOUNTERED.
5. TAPERS SHALL BE LOCATED TO MAXIMIZE THE VISIBILITY OF THEIR TOTAL LENGTH.



- TO BE DISPLAYED TWO WEEKS OUT FROM START OF WORK ROADWORK STARTING DECEMBER 18 (use actual calendar date)
TO BE DISPLAYED ONE WEEK OUT FROM START OF WORK ROADWORK STARTING MONDAY (use actual day of week)
TO BE DISPLAYED DURING ACTIVE PROJECT (WITH NO LANE CLOSURES) ROADWORK AHEAD PROCEED WITH CAUTION
TO BE DISPLAYED DURING ACTIVE PROJECT (WITH LANE CLOSURES) RIGHT LANE (use lane closed) CLOSED AHEAD PROCEED WITH CAUTION

ATLANTIC TRAFFIC+DESIGN logo with contact information for Warren, NJ and Chalfont, PA.

Table: REVISIONS. Columns include No., DATE, COMMENT, BY.

PROTECT YOURSELF logo with a warning sign and text: 'WHAT YOU DON'T KNOW CAN HURT YOU. THE STATE OF NEW JERSEY REQUIRES THE PROTECTION OF CONSTRUCTION WORKERS BY ANY PERSON PREPARING TO DISTURB THE EARTH'S SURFACE ANYWHERE IN THE STATE.'

Table with project details: PROJECT NO., DRAWN BY, CHECKED BY, DATE, SCALE, CAD ID.

McDonald's logo and project information for Highway Improvement Plans at McDonald's USA, LLC, including address and location in Burlington County, NJ.

NOT APPROVED FOR CONSTRUCTION

ATLANTIC TRAFFIC+DESIGN logo with contact information for Warren, NJ.

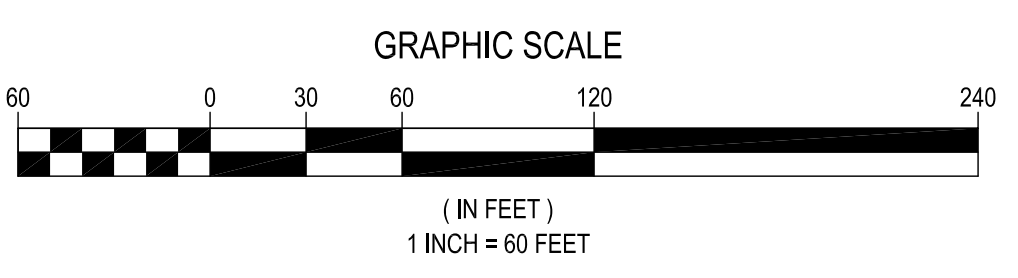
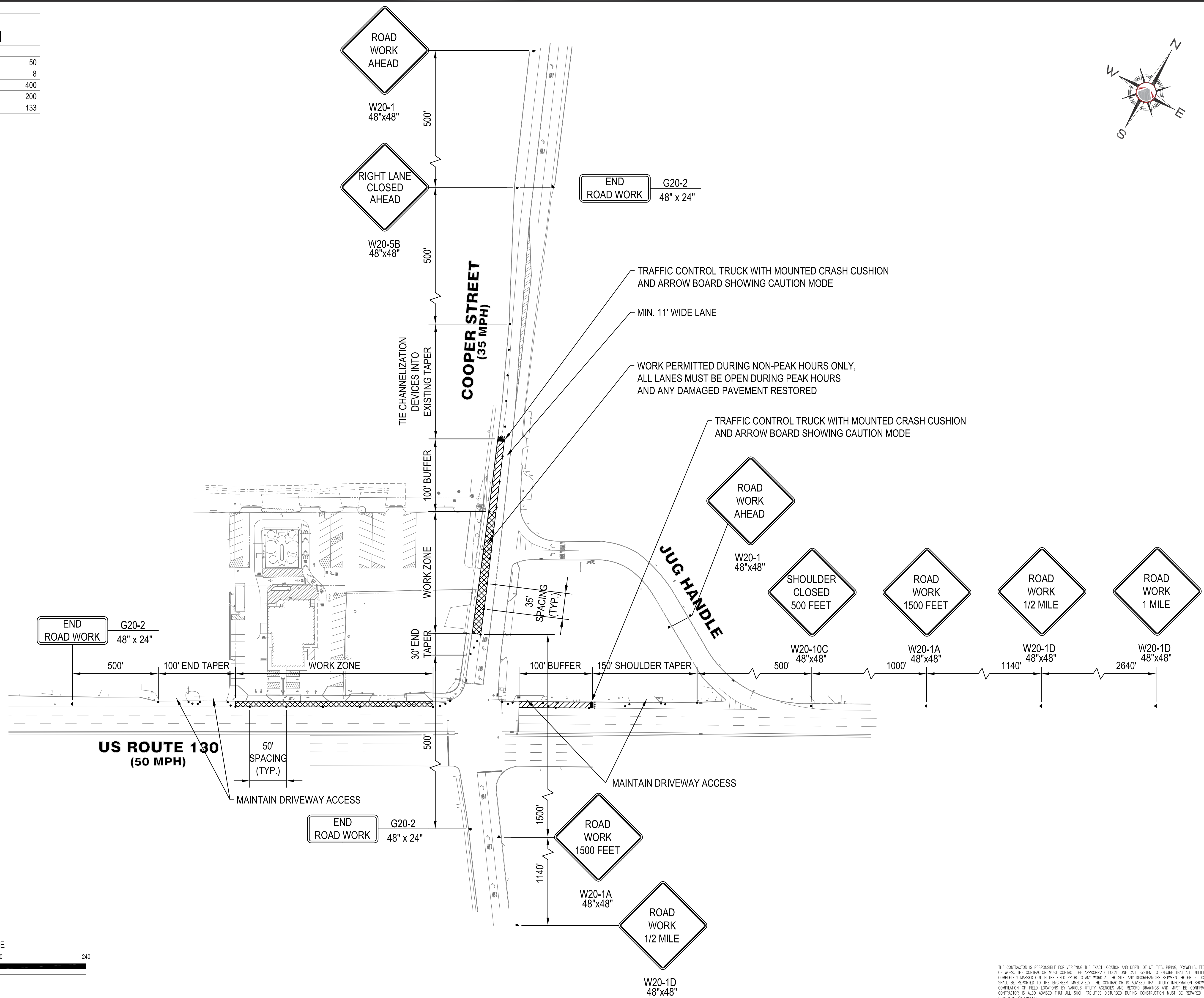
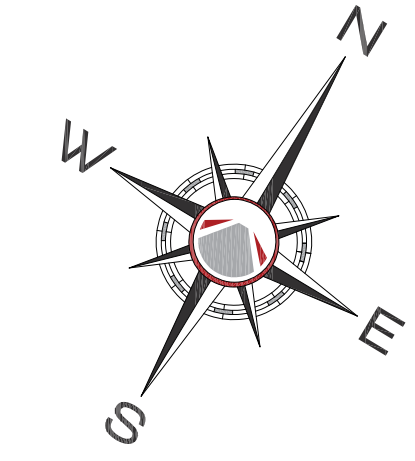
JOHN R. HARTER logo and professional engineer information: NEW JERSEY LICENSE NO. 41033.

Table: SHEET TITLE (TRAFFIC CONTROL NOTES), SHEET NUMBER (1 OF 2), and REVISION (0 4/10/2020).

Vertical text on the far left edge of the page containing drawing and revision information.

| M.U.T.C.D. MINIMUM TAPER LENGTHS*[FT] |     |
|---------------------------------------|-----|
| US ROUTE 130                          |     |
| SPEED LIMIT [MPH]                     | 50  |
| LATERAL SHIFT (W) [FT]                | 8   |
| L (LANE MERGE TAPER) [FT]             | 400 |
| L/2 (LANE SHIFT TAPER) [FT]           | 200 |
| L/3 (SHOULDER TAPER) [FT]             | 133 |

\* FOR S>=45MPH, L=WS



**ATLANTIC TRAFFIC+DESIGN**  
 Warren, New Jersey | Hightstown, New Jersey | Chalfont, Pennsylvania  
 The information herein is provided for informational purposes only and shall not be construed as a contract. The contractor is responsible for verifying the exact location and depth of utilities, piling, drywells, etc. prior to the start of work. The contractor must contact the appropriate local one call system to ensure that all utilities are properly and completely marked out in the field prior to any work at the site. Any discrepancies between the field locations and the plans shall be reported to the engineer immediately. The contractor is advised that utility information shown on the plan is a compilation of field locations by various utility agencies and record drawings and must be confirmed accordingly. The contractor is also advised that all such facilities disturbed during construction must be repaired or replaced at the contractor's expense.

| REVISIONS |      |         |    |
|-----------|------|---------|----|
| No.       | DATE | COMMENT | BY |
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**PROTECT YOURSELF**

WHAT YOU DON'T KNOW CAN HURT YOU.  
 THE STATE OF NEW JERSEY REQUIRES THE USE OF UNDERGROUND UTILITY LOCATOR (U.L.O.) CALL BEFORE YOU DIG.

PROJECT No.: AJ17240  
 DRAWN BY: JZ  
 CHECKED BY: JH  
 DATE: 4/10/2020  
 SCALE: 1" = 60'  
 CAD ID.: ANJ17240\_TCP

PROJECT:  
**HIGHWAY IMPROVEMENT PLANS**  
 FOR  
**McDONALD'S USA, LLC**

4295 & 4299 US ROUTE 130  
 EDGEWATER PARK TOWNSHIP  
 BURLINGTON COUNTY, NEW JERSEY

NOT APPROVED FOR CONSTRUCTION

**ATLANTIC TRAFFIC+DESIGN**  
 35 Technology Drive  
 Warren, New Jersey 07059  
 908-769-5588  
 NJ Certificate of Authorization No. 24GAZ27957900

**JOHN R. HARTER**  
 PROFESSIONAL ENGINEER  
 NEW JERSEY LICENSE NO. 41033

SHEET TITLE:  
**TRAFFIC CONTROL PLANS**

SHEET NUMBER:  
**2**  
 OF 2

REVISION 0 4/10/2020